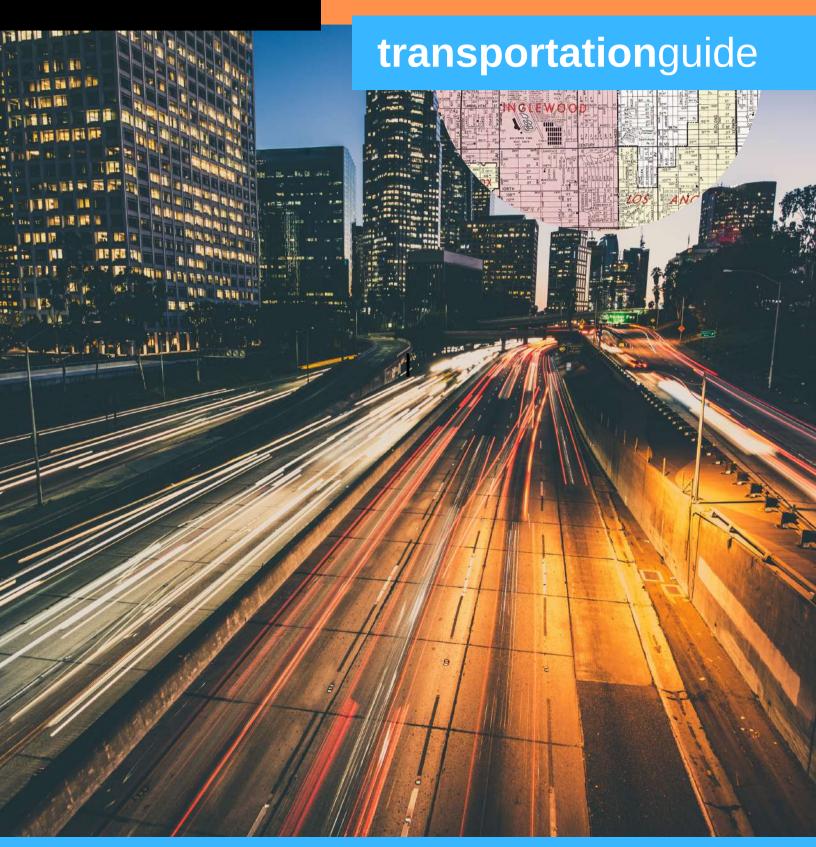
The CORO LeadLA Guide

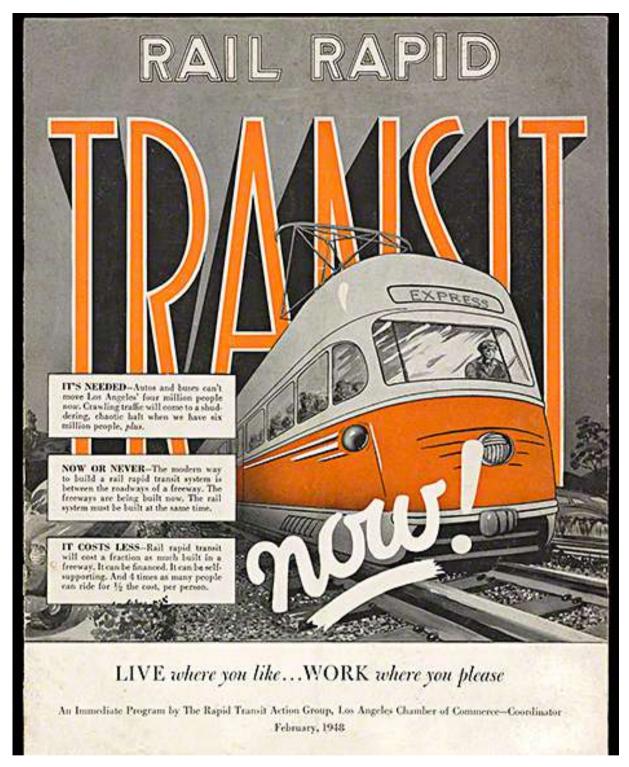
Los Angeles County







"It is widely assumed that Americans' infatuation with cars led to the construction of America's superhighways. But actually when Congress passed the Interstate Highway Act in **1956**, car sales were slack, and there was no popular clamor for building a new road system. At the time only about half of American families owned an automobile; everyone else depended on public transportation. Congress was responding to aggressive lobbying by auto makers and road builders, plus realtors who saw profits in developing suburban subdivisions"

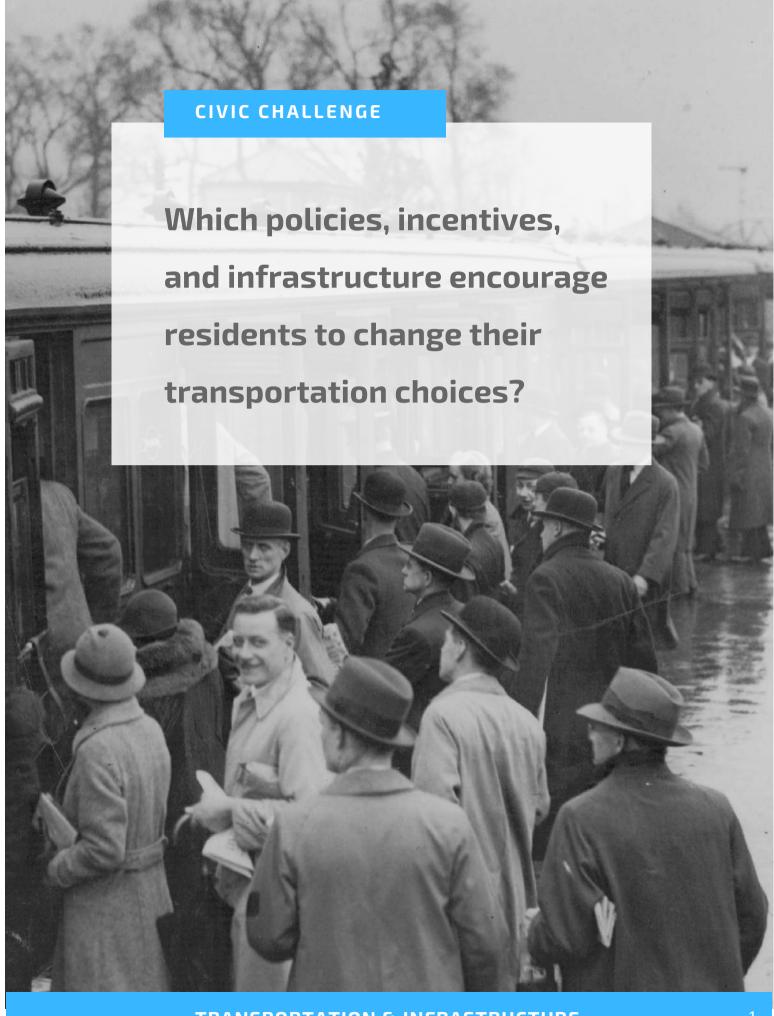


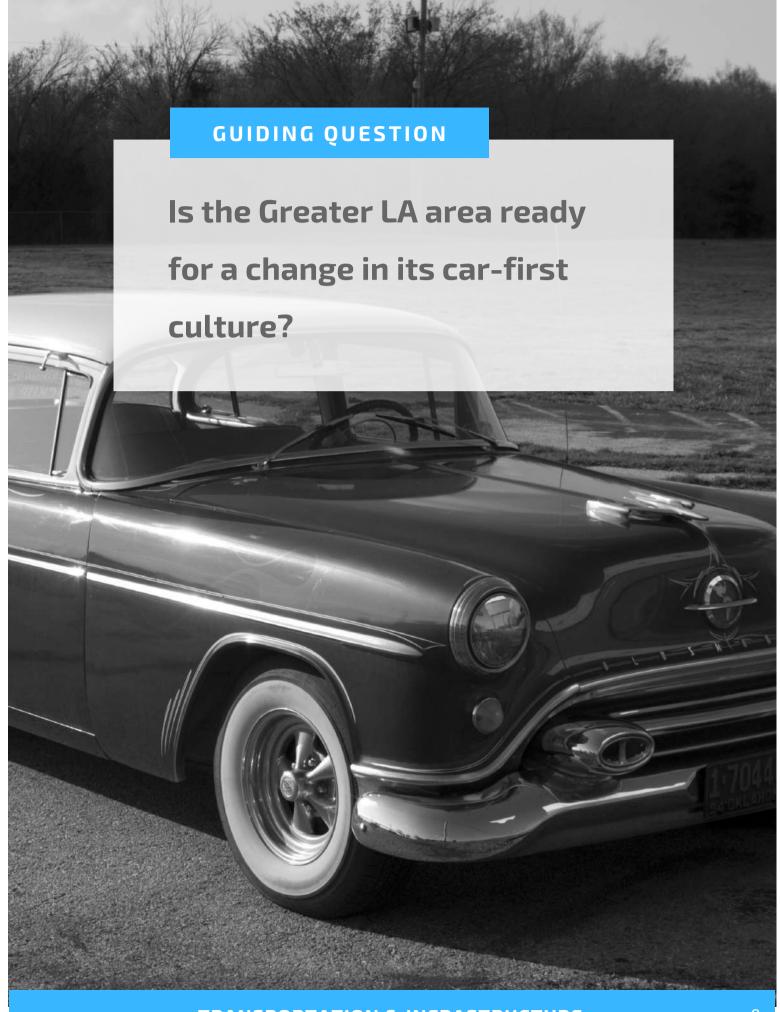
Rail Rapid Transit Now!, report by the Rapid Transit Action Group, Los Angeles Chamber of Commerce, 1948. Ink on paper, 11 x 8 5/8 in. (28 x 21.9 cm). Los Angeles County Metropolitan Transportation Authority Library & Archive

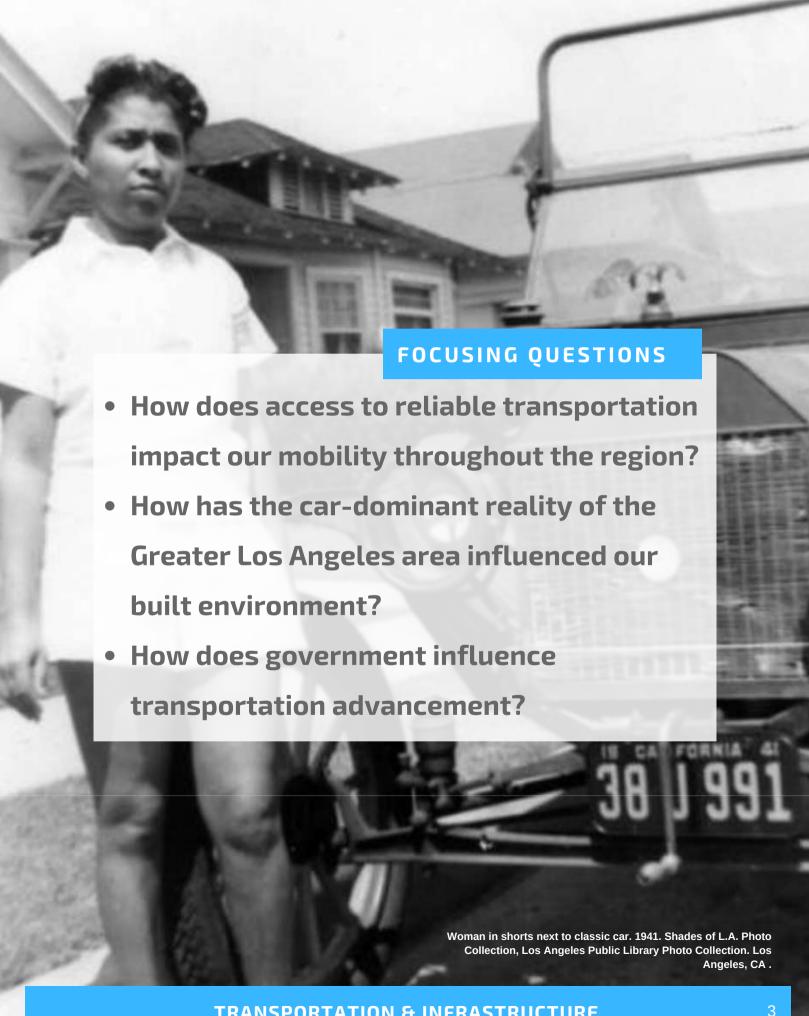
"With their cars, Angelenos go places, they travel infinite numbers of kilometers in a world that continuously remains Los Angeles."

—Cees Nooteboom

You name it, grandma, 73, will take a crack at it. 1961. Valley Times Collection, Los Angeles Photo Collection. Los Angeles, CA. (HCNVT_d031_f34_i17)







Los Angeles has not always been the automobile capital of the world.

The post-war housing boom of the late 1940s and early 1950s saw a decentralization of housing in the Los Angeles region. This, along with the development of our current **freeway system**, was responsible for making transportation in our vast region dependent on a **private automobile as a primary mode of transportation**. As cars became more popular and our freeway system was nearing completion, **the streetcar system**, **which once covered 1,100 miles with 900 cars**, shrank and eventually closed down.

However, in the last few decades, as our region's **population has boomed** without a commensurate boom in housing densification thereby pushing out commutes and as people have become more environmentally conscious, many have begun to look for alternatives to being boxed in a car. **Public transportation and alternative modes have become politically popular**—and possibly necessary for our economy to boom, again.

Even as there is talk of **alternatives to single automobile transportation**, it is still the primary mode for most households in Los Angeles County, with **73% of people driving a car solo to and from work** versus only about 7% using public transportation.

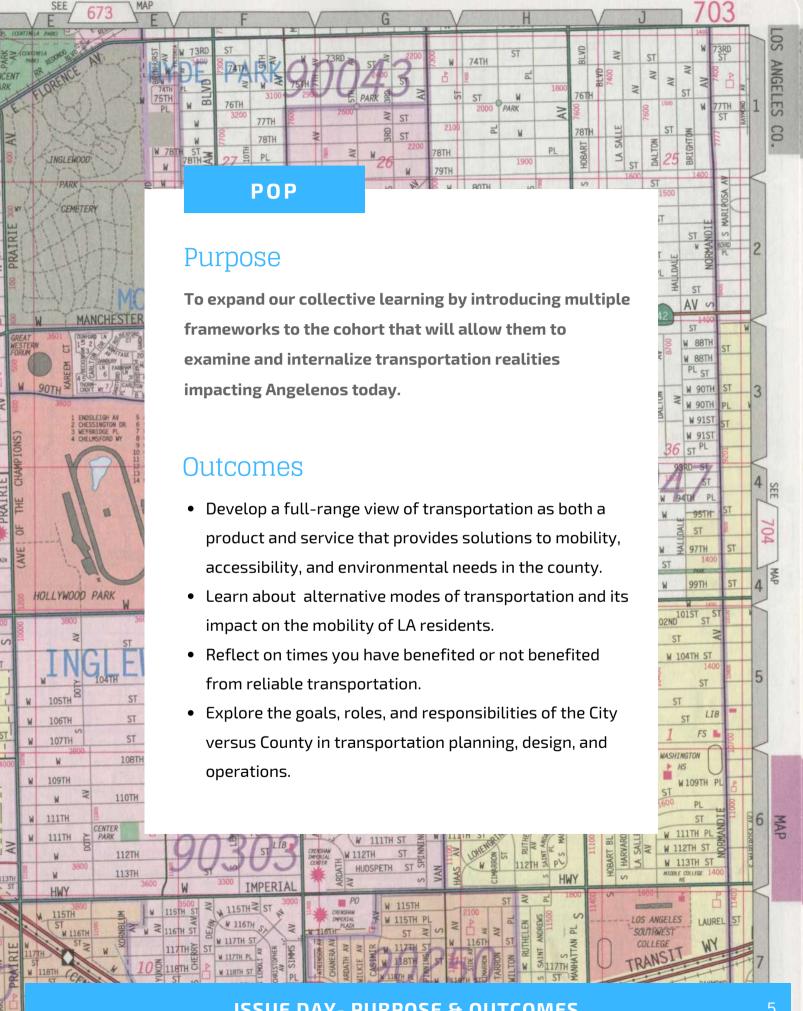
The Los Angeles County Metropolitan Transportation Authority (LA Metro) operates the third largest public transportation system in the United States. With 93 stations and nearly 100 miles of service, LA Metro is responsible for roughly 70% of all the public transit system in the County. Through the initial passage of Measure R in 2008 to fund public transportation projects and the expansion of this funding through Measure M which passed in 2016, LA Metro is well resourced to expand its transportation infrastructure. It's bus and rail system already deliver more than 1.2 million passenger rides on a typical weekday, and though it is expanding its accessibility and availability in more neighborhoods now than ever before, it is facing a decline in public transit ridership and is having a difficult time incentivizing and encouraging public transportation use. Transit ridership in Los Angeles has declined more than 19% since 2013.

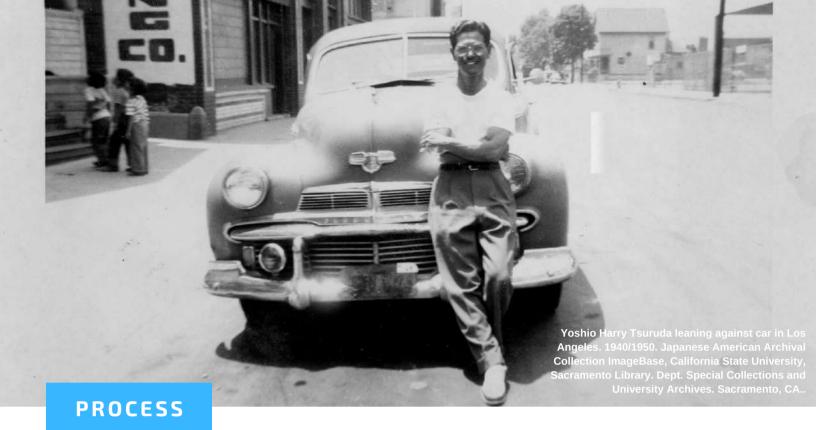
LA Metro has launched a major campaign called **Twenty-Eight by '28** with projects that include the Downtown Regional Connector, the Westside Subway Extension, and the Crenshaw Line to expand service, as well as cycling infrastructure, bike share expansion, sidewalk repair, and other enhancements to encourage alternative forms of transportation.

And no longer is the transportation debate merely about a car versus public transit (or bike). More recently, **disruptive transportation companies like Bird, Lime, Lyft, Uber** and others are changing the way that consumers and policy-makers are thinking and talking about transportation--especially the very crucial **'first mile/last mile'**. As their usage and demand grow, cities will be forced to think about how they will design their transit to more safely accommodate them. (Remember: 'transit' includes sidewalks).

Some might say that Los Angeles is too big, too diverse, and too populated to tackle the issue of seamless and integrated transportation options. Others might point back to when the streetcar reigned as king in Los Angeles as an example of the region's ability to evolve with the needs and wants of its residents. Yet all will acknowledge that **transportation facilitates the access to and flow of our lives** and therefore, intersects with far too many other issues to not be on the forefront of discussion.

For more background information refer to our bibliography: bit.ly/TeamTranspoArticles





8:30AM - 9:00AM Arrival & Breakfast

9:00AM - 9:30AM Opening Activity

9:30AM - 10:30AM Morning Session:

Nolan Borgman, Senior Transportation Planner - Metro, & Naomi Iwasaki, Deputy Director of Investing In Place

10:30AM - 2:00PM Travel, Lunch, and Experiential Visits

Inglewood: with Carson & Alia Downtown: with Heath & John Westwood: with Jennie & Amy

2:00PM - 2:30PM Group share-out on experiences, LATTC

2:35PM - 3:45PM Afternoon Session:

Jay Kim, Assistant GM - Mobility Management LADOT

3:45PM - 4:00PM Break

4:00PM - 5:00PM Debrief & Closing

5:00PM - 7:00PM Happy Hour at First Taproom Bar & Grill

Biographies

Nolan Borgman

Senior Transportation Planner, Office of Extraordinary Innovation, Metro

Nolan Borgman is a Sr. Transportation Planner managing Metro's Unsolicited Proposal process and Headquarters Ride Along program. Prior to joining OEI, Nolan worked in the Office of the CEO, Management Audit Services Department and was a Coro Fellow. He is invested in improving the customer experience. Projects in implementation and/or development include roofing and solar P3; Los Angeles Aerial Rapid Transit; streamlined enforcement of Wilshire peak bus lanes; mobile tolling; smart bike rack; network of mobile charger vending machines; enterprise cyber security project; artificial ivy graffiti abatement; and a vehicle to infrastructure application to improve performance on the Metro Orange Line.

Naomi Iwasaki

Deputy Director. Investing in Place

Naomi Iwasaki has direct experience translating community perspectives into real policy and projects. Driven by social justice, she is a fourth generation Angeleno with a love and deep connection to the Los Angeles region and those who call it home. Most recently, Naomi served as Director of Neighborhood Services and Great Streets in the Office of LA Mayor Eric Garcetti, overseeing eight City departments and the Great Streets Initiative, which prioritizes public resources to develop our streets as safe and vibrant public spaces. She previously worked at Community Health Councils (CHC) analyzing transportation and transit-oriented development policies to advocate for improved health and mobility access for residents of South Los Angeles. Naomi holds a Master's Degree in Urban Planning from New York University and B.A.'s in Ethnic Studies and Social Welfare from UC Berkeley.

Investing in Place believes that families can't get ahead in life if they can't get around, which is why their core mission is to support transportation investments that strengthen communities. Since January 2015, Investing in Place has convened diverse stakeholders and decision-makers through quarterly convenings, educated partners with real-time and accessible analysis on key transportation policies, provided technical assistance to stakeholders, and helped influence significant policies like Los Angeles County's Measure M, Metro's Active Transportation Strategic Plan, City of Los Angeles' sidewalk repair policy, and Metro's First and Last Mile policy.

Biographies

Jay Kim

Assistant General Manager, LADOT

Jay W. Kim is an Assistant General Manager at the Los Angeles Department of Transportation (LADOT). He currently oversees the Office of Mobility Management and provides executive management oversight for the planning and operation of transit, paratransit, development review, on and off-street parking facilities and taxicab regulations to ensure that LADOT meets the goals of the Sustainable City Plan. He has 29 years of transportation planning and engineering experience from both private and public sectors.

In addition, he has a long history of collaborative efforts with other City Departments and outside agencies to develop long-term transportation strategies, programs and projects to provide innovative urban mobility solutions in a digital age for the City of Los Angeles.

Mr. Kim is a past Chair of the LADOT Complete Streets Committee along with active past participation in the Integrated Mobility Hubs Working Group, Downtown Street Standards Committee and Metro's Congestion Mitigation Fee Policy Advisory Committee. Mr. Kim is a registered Professional Traffic Engineer in California and a member of the Institute of Transportation Engineers (ITE). He holds an Engineering degree from UCLA.





Manchester Ave Inglewood San Diego Freeway opening, Inglewood, California. 10 June 1963. Collection of Inglewood Local History Photographs, Inglewood Public Library

Purpose: To discover how our built environment responds to and determines transportation outcomes; to see how transportation plays an important role in economic development of a region/community; to understand equity in terms of transportation and how mobility decisions can bolster or inhibit a community.

Outcomes: To gain a better understanding of the process in which infrastructure projects consider transportation outcomes and mobility within and around the community, and to better explore the ramifications of such mobility decisions for the community surrounding the infrastructure development and the greater region.

Process: Engage with the City of Inglewood Economic Development Department and the City of Inglewood's mobility consultant in a vantage point discussion on the Rams Stadium construction in Inglewood, California. This discussion will be followed by a no-host lunch that will provide an experiential component exploring transportation equity in action.

Getting there & away: combination of Expo, bus, and TNC

Inglewood Travel Guides

Christopher Jackson

Director, Economic and Community Development Department, City of Inglewood

Christopher E Jackson, Sr., has been employed with the City of Inglewood since July 2008. Starting as a Planner in the Planning Division, he advanced within the city and was appointed as the Department Manager in June 2015. As a planner with the City of Inglewood, Mr. Jackson was handpicked to lead the city team to process the discretionary actions concerning the major renovation to the Forum and the Hollywood Park Tomorrow project. Most recently he was the lead planner for the City of Champions Revitalization Initiative which resulted in the approval to construct a NFL spec stadium and performance venue to be located within the Hollywood Park Tomorrow project area.

Lisa Trifiletti

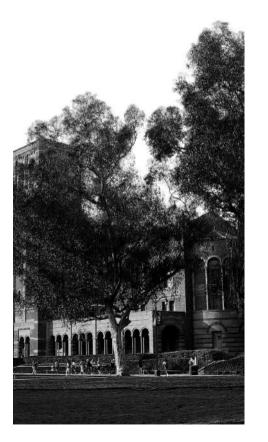
Principal, Trifiletti Consulting

On October 1, 2016, Lisa Trifiletti launched her own consulting firm to provide strategic land use, environmental, entitlement, real estate, transportation, project management, and governmental/public outreach consulting for private sector development projects and governmental agencies. Her experience with complex public infrastructure, transportation projects, and updates of governmental planning documents make her a known expert in the field. Lisa Trifiletti is currently serving as a strategic advisor to the Los Angeles World Airports (LAWA) on its environmental clearance and sustainability work at the Los Angeles International Airport (LAX), to the City of Los Angeles Department of City Planning on the update of the Southwest Valley Community Plan. In addition, some of her other projects include developing the City of Inglewood's transportation plans, the Sepulveda Pass Feasibility Study for Metro, and the revitalization of the Vermont/Manchester area for the office of County Supervisor Ridley-Thomas.

Other travel guides include: Sharon Mann Garrett from the City of Inglewood







Purpose: To understand the Expo Line and public rail from a planning and design perspective and to discuss electric scooters as first mile/last mile transportation options.

Outcomes: To understand public transportation design and engineering as it manifests in the real world; and to discuss Bird as an alternative form of transportation, its intended customer, and how it determines availability/access.

Process: Expo line ride-along with Greg Spotts and Alissa Walker. Followed by a discussion with Lys Mendez Palomo of Bird.

Lunch: No-host lunch and discussion at Lanny's Deli, Westwood

Getting there & away: Expo line return trip

Westwood Travel Guides

Greg Spotts

Assistant Director Chief Sustainability Officer, Los Angeles Bureau of Street Services

Greg currently oversees three large customer-facing divisions comprising more than 500 full time staff and more than \$50M in annual budget. His team responds to more than 130,000 constituent service requests per year: the Street Maintenance Division fixes potholes and provides motor sweeping, the Urban Forestry Division manages the care of 700,000 street trees, and the Investigation and Enforcement Division enforces codes pertaining to use of the streets and sidewalks. In addition, he oversees the Methods & Standards Division, which includes the PerformanceStat, Information Technology and Social Media functions.

Alissa Walker

Urbanism Editor, Curbed

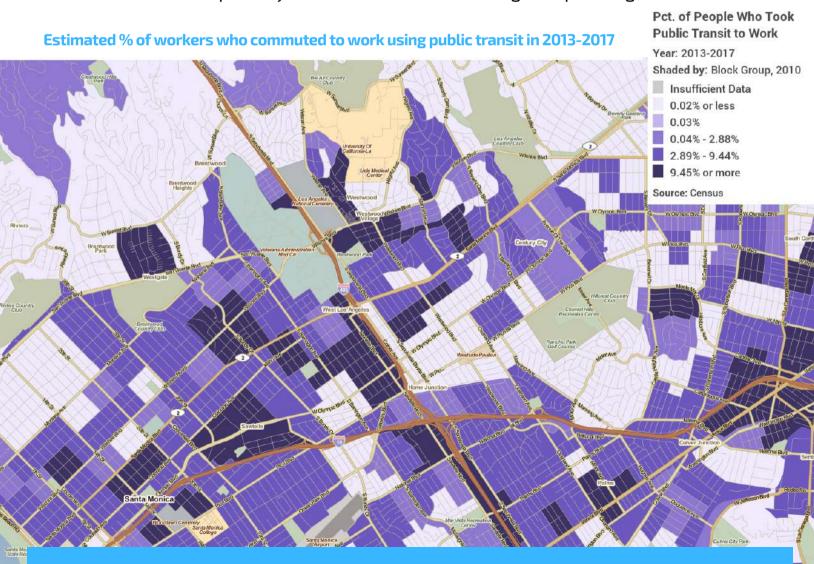
Alissa Walker connects people with where they live through writing, speaking, and walking. As the urbanism editor at Curbed, she authors the column Word on the Street, highlighting the pioneering transit, clever civic design, and game-changing policy affecting our cities. For her writing on design and urbanism, Alissa has been named a USC Annenberg/Getty Arts Journalism Fellow and Journalist of the Year by Streetsblog Los Angeles. In 2012 her project Good Ideas for Cities was selected for inclusion in the U.S. Pavilion at the Venice Architecture Biennale. In 2015 she received the Design Advocate award from the LA chapter of the American Institute of Architects. She is also the cofounder of design east of La Brea, a nonprofit that has received two National Endowment for the Arts grants supporting its LA design events. Alissa lives in Los Angeles, where she is a mom to the city's two most enthusiastic public transit riders.

Westwood Travel Guides, cont.

Lys Mendez Palomo

Government Partnerships, Bird

Lys Mendez is a Senior Manager on Bird's Government Partnerships team, working with local governments to integrate electric scooters as an effective last-mile solution in cities across Southern California. Lys is an urban planner and communications strategist, a lens that is critical to understanding mobility needs across cities and effectively communicating Bird's role in the transportation ecosystem. Lys has worked on mobility projects such as Los Angeles' Vision Zero program and 100 Hours, an effort by the Southern California Association of Governments to reimagine congestion solutions in the region. She also worked with CicLAvia, the popular roving open-streets event in Los Angeles County, and as a planning consultant for the City of Los Angeles to launch the Plan for a Healthy LA, which provided public health guidelines for the city's future growth and development. She worked for LA Mayor Antonio Villaraigosa, for the County of Riverside and as a newspaper reporter for the Riverside Press-Enterprise. Lys has an MA in urban and regional planning from UCLA.







Purpose: To engage in a private-public experience grounded in the interest of transportation. The focus is to consider design, accessibility, convenience, efficiency, and affordability that each sector provides to its stakeholders.

Outcomes: To gain an appreciation of the complexity of transportation, and see firsthand the lense by which different companies and organizations are approaching transportation improvements for various stakeholders.

Process: Visit and tour of Metro Command Center and Toyota dealership, supplemented by a conversation with Allen Narcisse from Lyft.

Lunch: Bag lunch

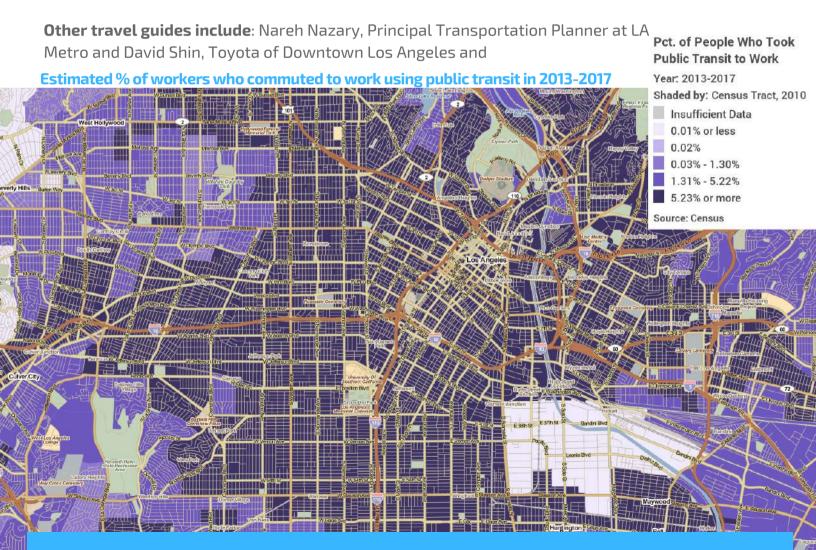
Getting there & away: Combination of public transportation & ride-hailing companies

Downtown Travel Guides

Allen Narcisse

Regional Director, Lyft

Allen is Regional Director of Southern California leading driver operations, driver service and marketing for territories including Los Angeles, Orange County, San Diego, Inland Empire, Santa Barbara and Bakersfield. Prior to Lyft, Allen was the first LA-based General Manager of UberEATS, Uber's food delivery operations, where he led the launch of Uber's first standalone app in the US and expansion to Los Angeles, San Diego, Orange County, Palm Springs and the Inland Empire. Before joining Uber, Allen co-founded Ebyline, a content marketing start-up, in 2009 and served as Chief Operating Officer. Allen earned a BBA with Distinction from the University of Michigan and an MBA from the Harvard Business School. A long-time resident of Los Angeles, Allen has been actively involved in community-based volunteering, including formerly serving on the KIPP LA Board, and coaching baseball at Ladera Height Little League.



Modes of Public Transportation

BUS RAPID TRANSIT: Express bus service characterized by the use of a dedicated roadway or restricted usage lanes, and a limited number of stops.

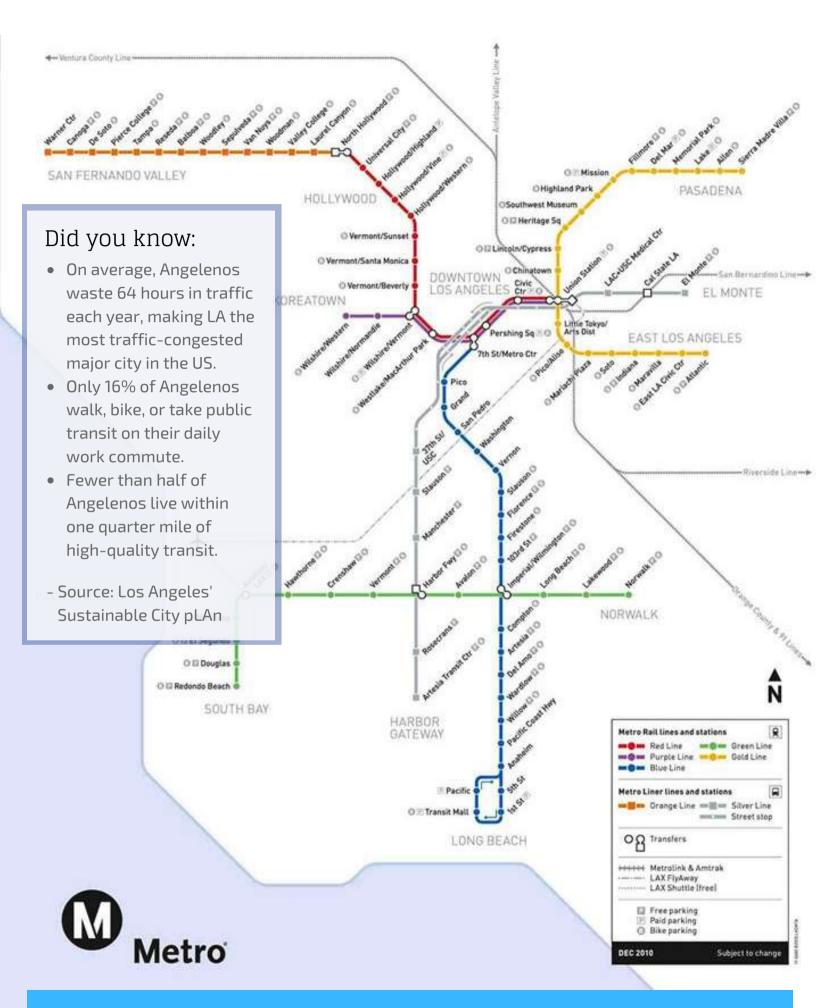
HEAVY RAIL (HR): An electric railway with capacity for heavy volume of traffic characterized by high speed and rapid acceleration passenger rail cars and by having an on-board operator. These transit systems (i.e., metro, subway, rapid transit, or rapid rail) are defined by their operation on a separated right-of-way, & use of a third rail on the ground to power the trains. Heavy rail systems require platform boarding, typically have longer distances between stations, & have greater capacity than Light Rail.

HIGH SPEED RAIL(HSR): Refers to a series of technologies involving trains traveling at top speeds of 90 to 300mph. This service may require separate, dedicated tracks and "sealed" corridors either through closure or through the construction of highway underpasses or overpasses.

LIGHT RAIL (LR): Usually driven by electric power taken from overhead lines, and usually, operates much, or all, of its route on non-exclusive right-of-way. A metropolitan-electric railway system characterized by its ability to operate in a variety of environments such as streets, subways, or elevated structures. LR operates in both exclusive and shared right-of-way environments, and have stricter limits on their length and the frequency of service than HR. Boarding takes place from streets or platforms.

MONORAIL: An electric railway of guided transit vehicles suspended from or straddle and/or guide-way formed by a single beam, rail, or tube.





Policies and Terminology

CONGESTION PRICING: Users get charged a fee that goes up or down based on demand for a limited commodity, in this case the roads.

Models for congestion pricing:

- The first, cordon pricing, is the most common in practice. It sets a variable fee to enter a certain neighborhood, usually the central business district, as is done in London and Stockholm.
- The second model charges drivers based on the number of miles they travel, with rates dependent on the time of day and where the travel occurred.
- Under the third model, drivers are charged on certain busy corridors, like freeways or big arterials, such as Wilshire Boulevard.

DOCKLESS MOBILITY: A term that refers to escooters, dockless bicycles and other transportation technologies that are not attached to a dock or station when not in use.

right first mile: An individual's trip is understood as the entire journey from origin to destination. Individuals may use a number of modes (types) of transport to complete the journey – they may walk, drive, ride a bicycle, take a train, or – in many cases – combine a number of modes. Bus and rail services often form the core of a trip, but users complete the first and last portion on their own. For example, they must first walk, bike, drive or roll themselves to and from the nearest station. This is referred to as the first and last mile of the user's trip, or "first/last mile" (FLM) for short. Actual distances vary by trip.

GAS TAX: Most states exempt gasoline from general sales taxes. However, several states do collect full or partial sales tax in addition to the excise tax. Sales tax is not reflected in the rates below.

MEASURE R: a half-cent sales tax increase that has been used to expand the Expo Line connecting downtown Los Angeles to Santa Monica, the Orange Line in the San Fernando Valley, the Silver Line connecting El Monte to the Harbor, and to expedite construction on the Green Line connection to LAX

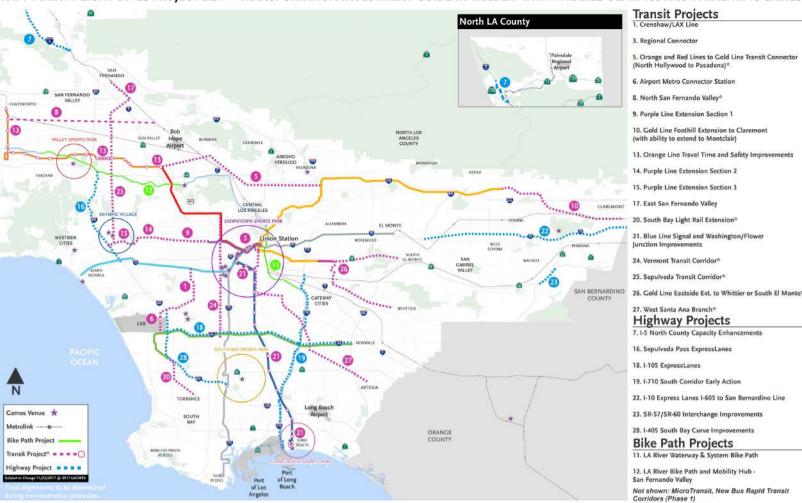
MEASURE M: (a permanent extension of Measure R) which will be used to expedite transit projects such as the Purple Line extension to UCLA, the Gold Line extension to Claremont and dozens of other projects.

Measure M provides \$860 billion in funding at a time when other crucial investments are being made in Parks, Housing, and Homelessness.

ROAD DIETING: Lane reduction is a technique in transportation planning whereby the number of travel lanes and/or effective width of the road is reduced in order to achieve systemic improvements.

For more terminology visit: bit.ly/TeamTranspoGlossary

DRAFT TWENTY-EIGHT BY '28 PROJECT LIST— TRANSPORTATION INVESTMENT GUIDE IN TANDEM WITH THE 2028 OLYMPICS AND PARALYMPIC GAMES





Transportation & Infrastructure Stakeholders

SCAG: Southern California Association for Government (SCAG), six-county SCAG region, which includes Imperial, Los Angeles, Orange, Riverside, San Bernardino, and Ventura

Metrolink: Metrolink is Southern California's regional passenger/commuter rail system serving over 55 stations across the region.

Mobility 21: Mobility 21 is a coalition that brings together public, business and community stakeholders to pursue regional solutions to the transportation challenges facing Imperial, Los Angeles, Orange, Riverside, San Bernardino, San Diego and Ventura counties. A nonpartisan alliance, Mobility 21 delivers a unified voice for the region's transportation priorities and seeks to improve mobility in the region.

L.A. County Federation of Labor, AFL-CIO, L.A./Orange County Building Trades and Construction Trades Council: The Los Angeles/Orange Counties Building and Construction Trades Council is in the business of creating good jobs and lifelong careers. It is an umbrella group representing 48 local unions and district councils in 14 Trades. Membership in these unions totals more than 100,000 skilled men and women.

Caltrans: California Department of Transporation is an executive department of the US state of California. The department is part of the cabinet-level California State Transportation Agency (CalSTA). Caltrans is headquartered in Sacramento.

Metro: The Los Angeles County Metropolitan Transportation Authority (branded as Metro; formerly branded as MTA or LACMTA) is an agency that operates public transportation in the Los Angeles metropolitan area

Move LA: Move LA in 2007 to bring together business, labor and environmental leaders and organizations with the goal of raising significant new funding for LA County's transit system. This coalition helped lead the campaign for the Measure R sales tax, and proved to be a powerful force in getting Measure R on the 2008 ballot and winning its passage, with the result that LA has embarked on an ambitious build-out of its transportation system.

LADOT: The Los Angeles Department of Transportation, commonly referred to as LADOT, is a municipal agency that oversees transportation planning, design, construction, maintenance and operations within the City of Los Angeles.

TNC: Ride-hail Transportation Network Companies (TNC, ie: Lyft and Uber)

LA County Board of Supervisors: The fivemember Board of Supervisors is the governing body of the County of Los Angeles. Created by the state Legislature in 1852, the Board has executive, legislative and quasijudicial roles. Members are elected by voters in their respective districts and are limited to three four-year terms.



Los Angeles County Metro Transportation Authority ("LA Metro") was formed in 1993 out of a merger between the Southern California Rapid Transit District and the Los Angeles County Transportation Commission. It is uniquely the **transportation planner and coordinator, designer, builder and operator** of LA County's public transit system.

- · 4 Light Rail, 2 subway
- · 93 Stations
- 98 Miles of Service
- · 165 Bus Routes
- · 2,308 Metro Bus Fleet



METRO'S BUS CUSTOMERS



Frequent Riders

Represent 7% of LA County residents but 80% of all Metro bus boardings



Occasional Riders

Over 40% would ride more if improvements were made



Infrequent Riders

Represent more than half of LA County



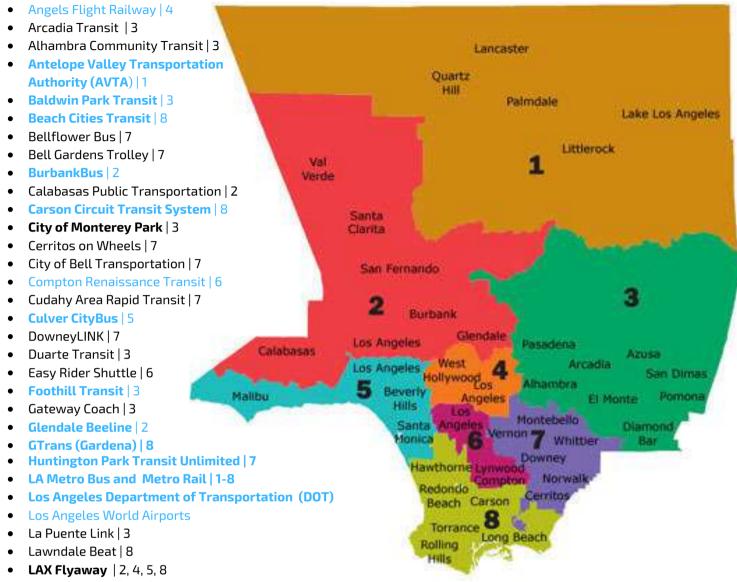
Non-Riders

If just 1 in 4 non-riders used transit two times per month, we could reverse declining ridership

Los Angeles County Transit Agencies

BOLD= The EZ transit pass is a monthly pass good for local travel on 23 different public transit carriers throughout the Greater LA region.

BLUE = All 25 transit systems that support the TAP card, which allows customers to use stored value, passes & single rides to travel in LA County



- Long Beach Transit | 8
- Lynwood Trolley | 6
- Metrolink and Amtrak | 1-8
- Montebello Bus Lines | 7
- Monrovia Transit | 3
- Norwalk Transit System | 7
- Palos Verde Valley Transit Agency | 8
- Pasadena Transit | 3
- Pomona Valley Transportation Authority | 3
- Rosemead Explorer | 3
- Santa Clarita Transit | 2
- San Fernando Trolley | 2
- Santa Monica Transit | 5
- Spirit Bus City of Montrey Park | 3
- Torrence Transit | 8
- Warner Center Shuttle | 2
- West Hollywood Cityline | 4

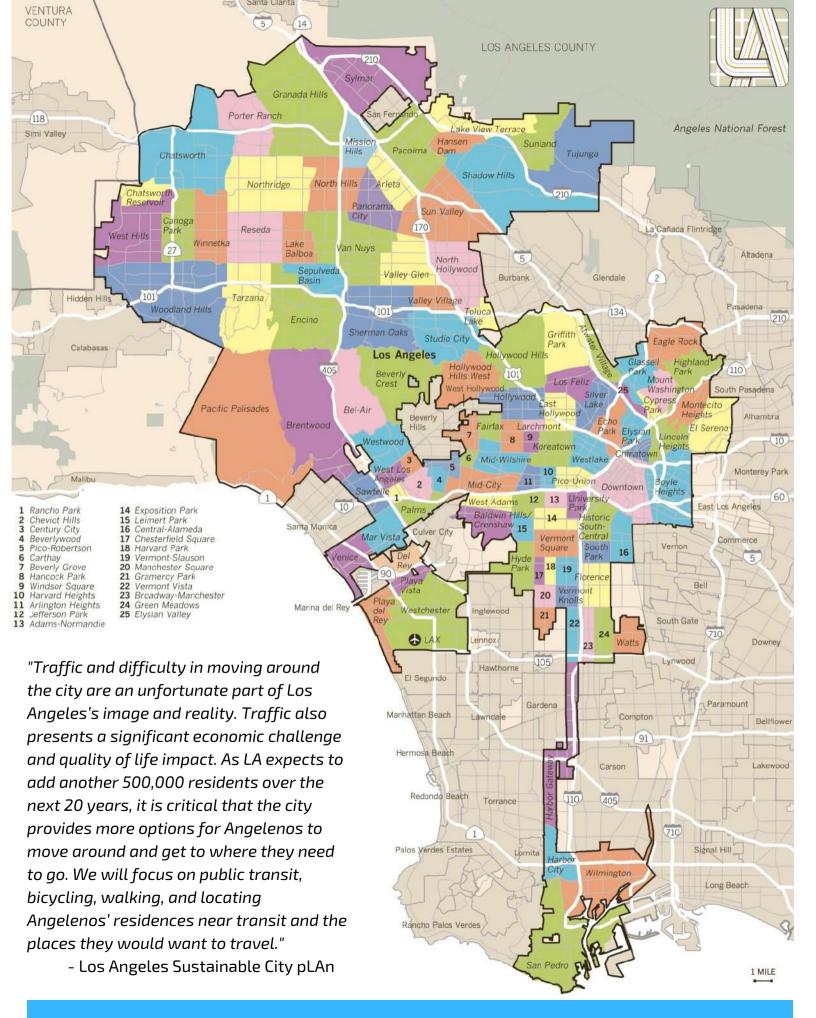
Service Provider Areas (SPAs)

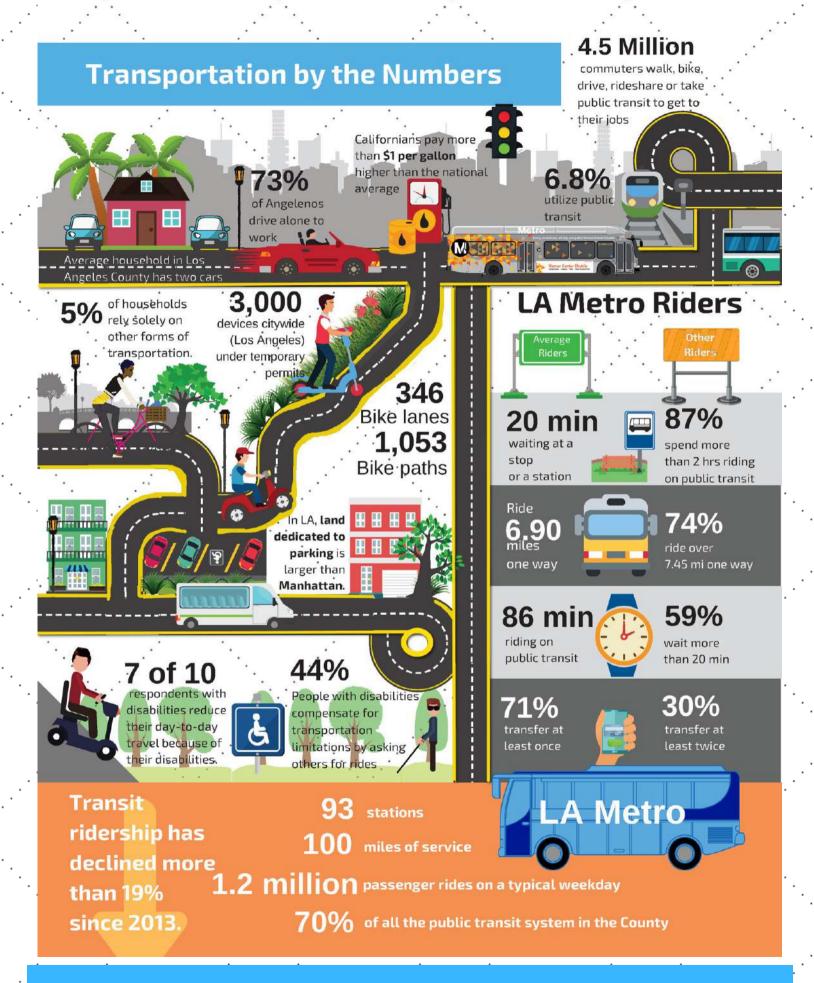
SA 1 (Antelope Valley) • SA 2 (San Fernando)
SA 3 (San Gabriel) • SA 4 (Metro) • SA 5 (West)
SA 6 (South) • SA 7 (East) • SA 8 (South Bay/Harbor)

Los Angeles County Department of Public Works

Unincorporated County of Los Angeles Transit Services

- Acton/Agua Dulce | 1
- Avocado Heights/Bassett/West Valinda | 7
- East Valinda Shuttle | 3
- El Sol | 7
- Edmund D. Edelman Children's Court | 4
- Heights Hopper Shuttle | 7

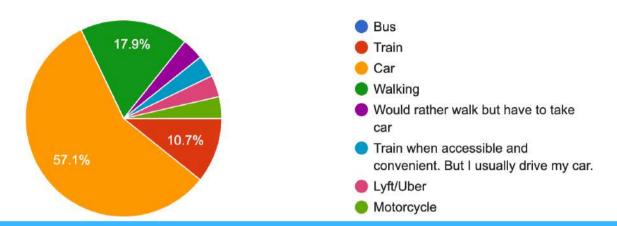




LEAD LA Survey Results

Which is your preferred mode of transportation?

28 responses



What is your typical length of trip?

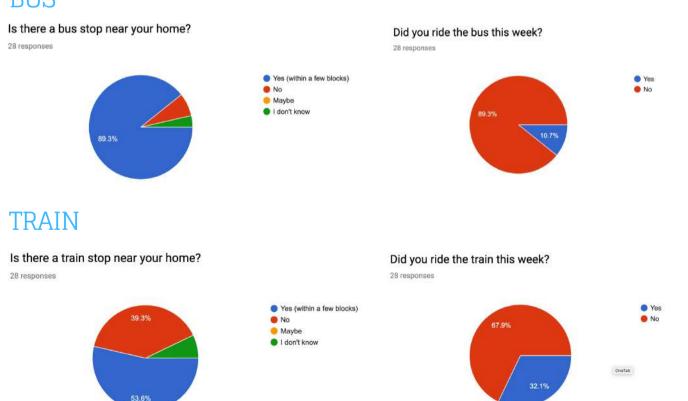
- 39.3% at less than 30 min
- 42.9 % at 31- 45 min
- 7.1% at 45 60 min
- 10.7% at 60+ min

92.9% of respondents own cars. Among those, there was an equal split between those who have 1 vehicle and those who have 2 vehicles (48.1%). 3.8% own 3 vehicles

Do you have a Tap card?

- 78.6% Yes
- 214% No

BUS





Tamika Butler, Toole Design
Nolan Borgman, LA Metro
Artie Fields, City of Inglewood
Nat Gale, Toole Design
Elissa Konove, Metro
Phil Law, SCAG
Jessica Meaney, Investing In Place
Mott Smith, Civic Enterprise Development
Jenny Vazquez-Newsum, Coro

And special thanks to our in-kind donors

Pepperdine School of Public Policy - Breakfast & PM snacks Rafael Salazar at LA Trade Tech for securing our homebase

Transportation Issue day team is made up of Carson Bruno, Lynnzi Brianza, Alia Kate, John Lulejian, Amy Amsterdam, Jennie Quinonez-Skinner, Heath Seltzer, Corey Matthews